



JACOBS



DRAFT MEMORANDUM 8: IMPLEMENTATION MEASURES (DRAFT)

ASTORIA UNIONTOWN REBORN MASTER PLAN

Attention: Mike Morgan, City of Astoria
From: Jamin Kimmell and Matt Hastie, APG
Date: June 26, 2019
Copies to: Michael Duncan, ODOT

The purpose of this memo is to specify amendments to the City's Development Code, Comprehensive Plan, and Land Use and Zoning Map to implement the preferred land use alternative for the Astoria Uniontown Reborn Master Plan, as identified in Draft Memorandum #7.

The preferred land use alternative includes adoption of a new overlay zone for a portion of the plan area in order to implement the land use vision for this area. A draft of the Uniontown Overlay (UTO) zone is presented in this memo. The UTO zone establishes new requirements or modifies existing standards related to allowed uses, setbacks, landscaping, building height, and design standards and guidelines.

The UTO zone is modeled on two other overlay zones—Bridge Vista Overlay Zone (BVO) and the Urban Core Overlay Zone (UCO)—in order to ensure that a consistent set of standards and requirements are applied to achieve similar goals in different areas of the City. Both overlay zones are currently being amended or created, so there is a need to track these changes and align the standards across the overlay zones, where needed to ensure consistency among the zones and with current city policy direction. This draft of the UTO is based on the following versions of these zones:

- Bridge Vista Overlay Zone, draft amendment pending review and potential adoption at the July 1, 2019 meeting of the Astoria City Council
- Urban Core Overlay Zone, draft amendments, dated April 16, 2019 (currently under review by City staff)

The revised and final versions of the UTO zone will be amended to align with these other overlay zones, as needed. This draft of the UTO zone includes placeholder graphics that will be replaced with more detailed graphics in the revised draft. There are also placeholders for section and figure numbers, which will be updated in the revised draft.

The Land Use and Zoning Map is proposed to be amended to include the boundaries of the UTO zone. The Comprehensive Plan is proposed to be amended to include background information on the plan and implementing policies.

ORDINANCE NO. __-

AN ORDINANCE AMENDING THE ASTORIA DEVELOPMENT CODE PERTAINING TO IMPLEMENTATION OF THE ASTORIA UNIONTOWN REBORN MASTER PLAN

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Astoria Development Code Sections 14.____ to 14.____pertaining to Uniontown Overlay Zone is hereby added to read as follows:

“UTO: UNIONTOWN OVERLAY ZONE

14.____ PURPOSE.

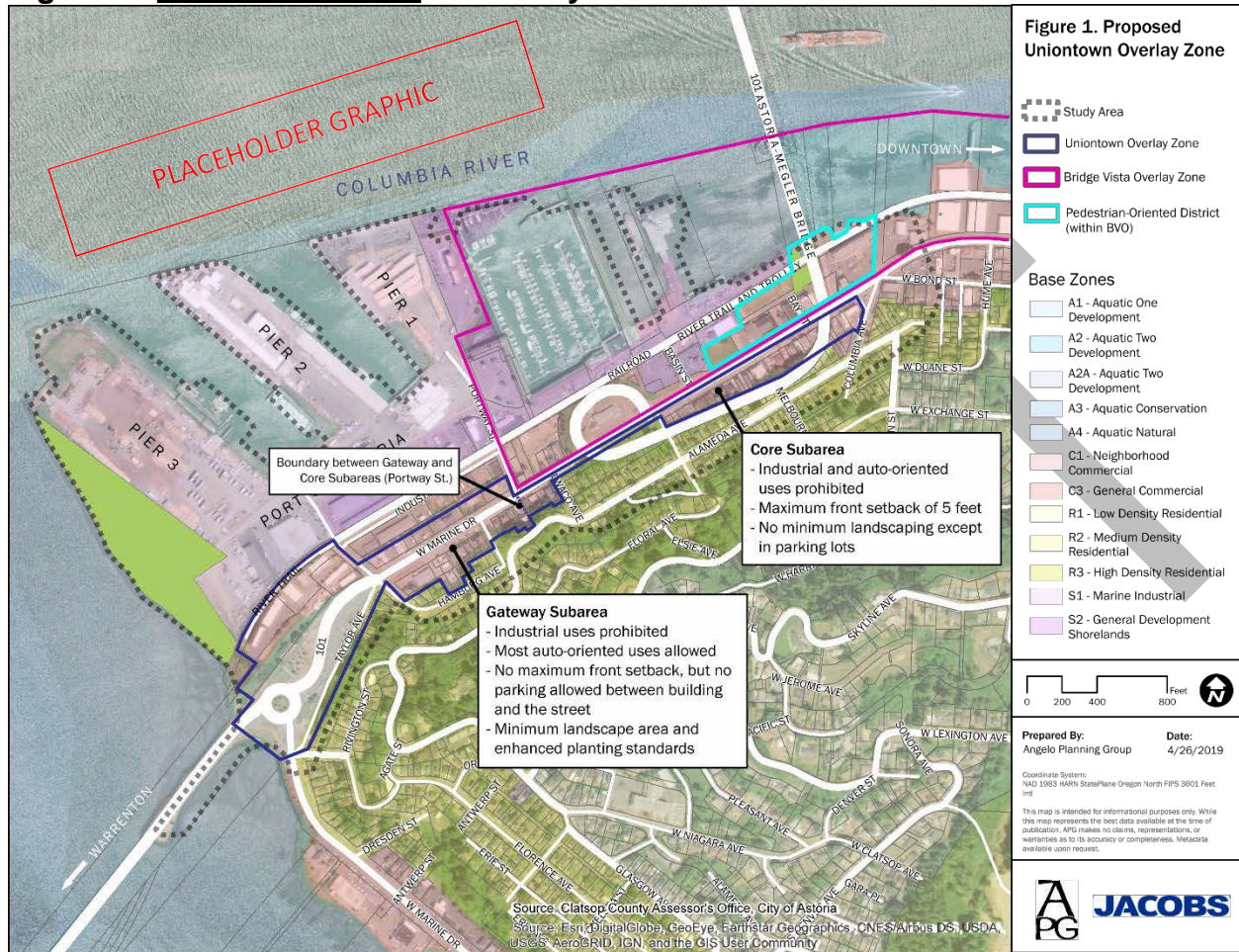
The purpose of the Uniontown Overlay Zone is to implement the land use principles of the Astoria Uniontown Reborn Master Plan, dated *[plan adoption date]* and address policy direction from the Astoria Planning Commission and City Council for this area. The Uniontown Overlay (UTO) Zone is intended to meet and balance multiple objectives, including creating an attractive western gateway into the City; developing a pedestrian-friendly commercial district; expanding the tree canopy and enhancing site landscaping; encouraging design of new or rehabilitated buildings that respects the character of the City and the Uniontown-Alameda National Register Historic District; and allowing a mix of uses that support a vibrant commercial corridor, new investment, and employment opportunities. The boundaries of the UTO Zone are depicted on the City's Zoning Map.

14.____ . APPLICABILITY AND REVIEW PROCEDURES.

A. Applicability

The provisions in Sections 14.____ to 14.____ apply to all uses in all areas of the Uniontown Overlay Zone unless indicated otherwise in the code.

Figure 14. -1: Overlay Zone Boundaries and Subareas



The provisions of the Uniontown Overlay Zone shall apply to all new construction or major renovation, where “major renovation” is defined as construction valued at 25% or more of the assessed value of the existing structure, unless otherwise specified by the provisions in this Section. Applications in the Uniontown Overlay Zone shall be reviewed in a public design review process subject to the standards and guidelines in Sections 14. -14. [all following sections of the UTO Zone].

B. Historic Design Review

When a development proposal is required to be reviewed by the Historic Landmarks Commission due to its proximity adjacent to a designated historic building, structure, site, or object, the Historic Landmarks Commission shall include review of the Uniontown Overlay sections relative to historic compatibility. If the proposed development is not "adjacent" to a historic property (as defined in Section 1.400) and not subject to review by the Historic Landmarks Commission, then the historic review of the Uniontown Overlay Zone shall be completed by the Design Review Commission.

14. _____. PERMITTED USES.

The following uses and activities and their accessory uses and activities are permitted outright in the Uniontown Overlay Zone, in addition to uses permitted outright in the base zone identified in Article 2, and subject to the other appropriate development provisions of this Section.

1. Existing motels and their expansion and reconstruction if destroyed.
2. Dwellings in a new or existing structure:
 - a. Located above or below the first floor with commercial facilities on the first floor of the structure.
 - b. Located in the rear of the first floor with commercial facilities in the front portion of the structure.
3. Light manufacturing with a retail component.
 - a. Facilities of maximum 2,000 square feet shall have a retail component of minimum 60 square feet;
 - b. Facilities greater than 2,000 square feet shall have a retail component of minimum 144 square feet.
4. Residential Home.
5. Residential Facility.

14. _____. USES PROHIBITED.

A. West Gateway Subarea.

The following uses and activities and their accessory uses and activities are prohibited in the West Gateway Subarea (Figure 14.____-1) in the Uniontown Overlay Zone. Permitted uses are identified in the base zones in Article 2 and in Section 14.105.A of this ordinance.

1. Light manufacturing without a retail component.
2. Communication service establishment.
3. Construction service establishment.
4. Transportation service establishment.
5. Recycling establishment.
6. Wholesale trade or warehouse establishment.
7. Motel, hotel, bed and breakfast, inn or other tourist lodging facility and associated uses

B. Core Subarea.

The following uses and activities and their accessory uses and activities are prohibited in the Core Subarea (Figure 14.____-1) in the Uniontown Overlay Zone. Permitted uses are identified in the base zones in Article 2 and in Section 14.105.A of this ordinance.

1. Auto sales and services.
2. Drive-through facilities.
3. Gasoline services stations.
4. Repair service establishment not allowed as an Outright Use.
5. Light manufacturing without a retail component.
6. Construction service establishment.
7. Communication service establishment.
8. Transportation service establishment.
9. Recycling establishment.
10. Wholesale trade or warehouse establishment.
11. Motel, hotel, bed and breakfast, inn or other tourist lodging facility and associated uses

14.____ DEVELOPMENT STANDARDS.

The following development standards apply to development in the Uniontown Overlay Zone.

A. Height.

1. Maximum building height is 35 feet except as noted in subsection (2) of this section.
2. Building height up to 45 feet is permitted when building stories above 28 feet are stepped back at least 10 feet in accordance with Section 14.____. [stepback section].
3. Exceptions to building height restrictions may be granted through provisions in Section 3.075.

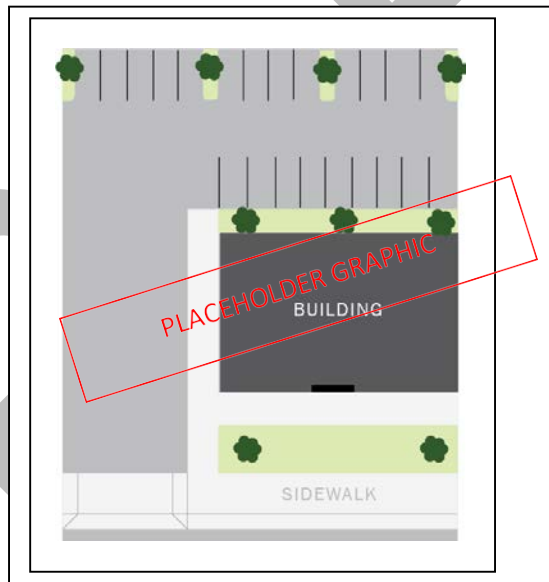
B. Setbacks.

Setback standards apply only to new development approved as of January 1, 2019 or additions to existing buildings.

1. West Gateway Subarea.
 - a. No minimum or maximum front setback standards apply to developments in the West Gateway Subarea.
 - b. Where buildings are set back from the street more than 5 feet, the setback area:

- 1) Shall be landscaped according to the standards of Section 14.____. [landscaping standards section]; and/or
 - 2) Shall include a pedestrian walkway, plaza, courtyard, or other pedestrian-oriented amenity or public gathering space (see Figure 14.____-).
- c. Adjacent to the River Trail.
- 1) The minimum setback adjacent to the River Trail shall be 10 feet on the south side of the trail
 - 2) The setback area shall be landscaped according to the standards of Section 14.____. [landscaping standards section]; and/or shall include a pedestrian walkway, plaza, courtyard, or other pedestrian-oriented amenity or public gathering space.

Figure 14.- : Building Setbacks in the West Gateway Subarea

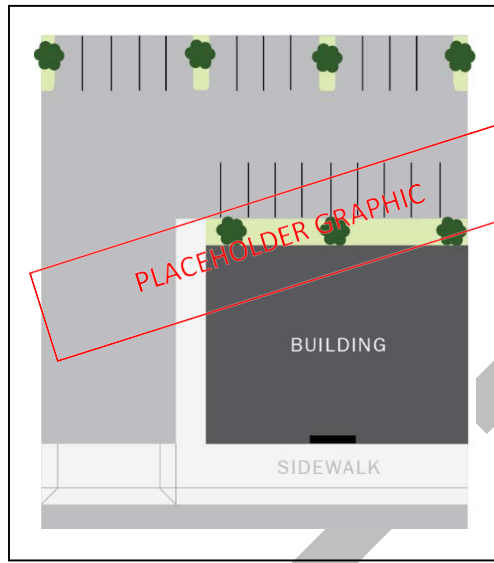


2. Core Subarea.

- a. The maximum setback for yards fronting W Marine Drive in the Uniontown Overlay Zone shall be five (5) feet (see Figure 14.____-).
- b. Allowed Extensions of Maximum Setbacks.

The maximum setback for yards fronting a public right-of-way in the Uniontown Overlay Zone may be extended to 20 feet for up to 50% of the building facade if the setback is used for a walkway, plaza, courtyard, or other pedestrian-oriented amenity or public gathering space.

Figure 14.____-_: Building Setbacks in the Core Subarea



C. Stepbacks.

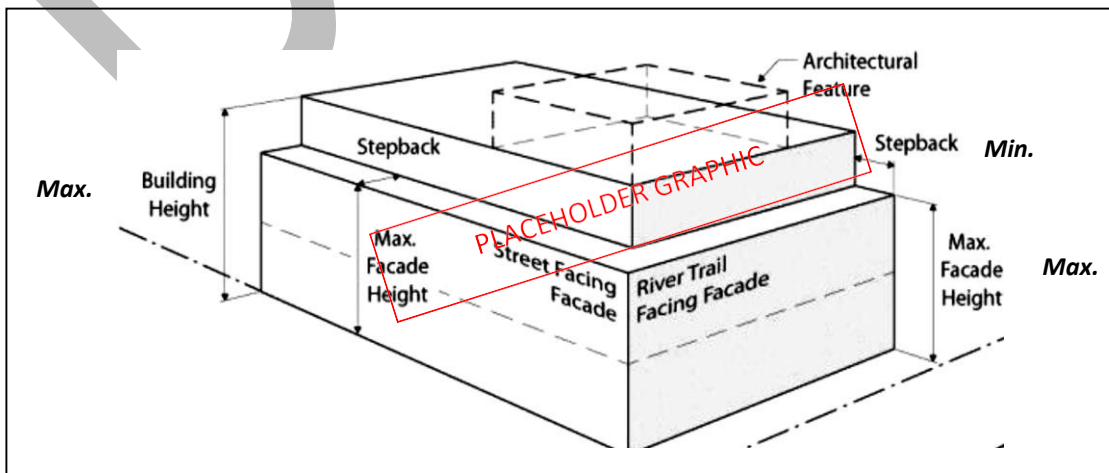
1. Purpose.

The purpose of a stepback is to allow for less obstructed views from above the building and to create a less imposing building scale as viewed from the right-of-way or parallel/adjacent trail. A stepback is also designed to allow more light down to the adjacent or fronting right-of-way, sidewalk, or trail.

2. Additional Building Height.

Where the height of a building or building addition is proposed to exceed 35 feet, at least that portion of the building exceeding 28 feet or two stories, whichever is less, shall provide a stepback of at least 10 feet from the plane of the proposed building or building addition that faces the right-of-way or River Trail (see Figure 14.____-).

Figure 14.____-_: Building Stepbacks



14. _____ DESIGN STANDARDS AND GUIDELINES

A. Applicability and Review.

The following design standards and guidelines apply to all new construction or major renovation, where “major renovation” is defined as construction valued at 25% or more of the assessed value of the existing structure. Applications in the Uniontown Overlay Zone shall be reviewed in a public design review process subject to the standards and guidelines in Sections 14. _____ -14. _____ [all sections of the UTO Zone except for the first two sections].

Some of the following design standards and guidelines apply to all uses. Other standards and guidelines are differentiated by non-industrial uses and industrial uses. For the purposes of these Sections, industrial uses include the following as further defined in Section 1.400 of the Development Code:

1. Light manufacturing with a retail component.

Non-industrial uses include all other uses that are permitted outright or conditionally in the C-3 zone in the Uniontown Overlay Zone.

B. Building Style and Form.

1. Standards for All Uses.
 - a. Projecting wall-mounted mechanical units are prohibited where they are visible from a public right-of-way or the River Trail. Projecting wall-mounted mechanical units are allowed where they are not visible from a public right-of-way or River Trail.
 - b. Solid waste disposal, outdoor storage, and utility and mechanical equipment shall be enclosed and screened from view (Figure 14. - _). A cover shall be required if screened items can be viewed from above. Rooftop equipment shall be screened from view by a parapet wall, a screen made of a primary exterior finish building material used elsewhere on the building, or by a setback such that it is not visible from adjacent properties and rights-of-way up to approximately 100 feet away. Also see Section 3.215, Outdoor Storage Areas and Enclosures.

Figure 14.____-_: Screening Waste Disposal, Outdoor Storage, and Utility/Mechanical Equipment



Examples of recommended solid waste disposal area and mechanical equipment enclosures.

2. Guidelines for All New Construction.

- a. The design of new construction should respect significant original characteristics, scale and massing of adjacent structures that are visible from the public right-of-way within three blocks of the development site. Buildings should be designed so that they are not substantially different in character from adjacent structures, in terms of size, mass, or architectural form. Also see Section 14.002.C, Resolving Conflicts within the Code.
- b. New construction should respect significant characteristics of composition and material of adjacent structures that are visible from the public right-of-way within three blocks of the development site. Also see Section 14.002.C, Resolving Conflicts within the Code.
- c. Building forms should be simple single geometric shapes, e.g. square, rectangular, triangular (Figure 14.____-_).

Figure 14.____-_: Geometric Building Form



3. Guidelines for All Existing Buildings.

- a. Distinctive stylistic features or examples of skilled craftsmanship of existing buildings and/or structures proposed for renovation, alteration, and/or additions should be treated with sensitivity. All

buildings should be respected and recognized as products of their time.

- b. Renovations, alterations, and/or additions to existing buildings should respect significant original characteristics of adjacent structure scale and massing for the entire structure, and should be designed so that they are not substantially different in terms of size, mass, or architectural form. Also see Section 14.002.C, Resolving Conflicts within the Code.
- c. Renovations, alterations, and/or additions should retain and/or respect significant original characteristics of the existing structure composition and material, for the entire structure. Also see Section 14.002.C, Resolving Conflicts within the Code.
- d. Building forms should be simple single geometric shapes, e.g. square, rectangular, triangular (Figure 14.____-__).
- e. Mid-century “slip covers” which are not part of the original historic design should be removed when possible.
- f. Incompatible additions or building alterations using contemporary materials, forms, or colors on building facades are discouraged.

4. Standards for Non-Industrial Uses.

a. Facade Variation.

All non-industrial buildings shall incorporate design features such as offsets, balconies, projections, window reveals, or other similar elements to preclude large expanses of uninterrupted building surfaces in areas which are visible to the public. Design features shall occur at a minimum of every 30 feet for all building facades visible from a public right-of-way or River Trail. (Figure 14.____-__)

The facade shall contain at least two (2) of the following features:

- 1) Recess (e.g., deck, patio, courtyard, entrance, or similar feature) that has a minimum depth of six (6) feet;
- 2) Extension (e.g., floor area, deck, patio, entrance, or similar feature) that projects a minimum of two (2) feet and runs horizontally for a minimum length of four (4) feet;
- 3) Offsets or breaks in roof elevation of two (2) feet or greater in height;
- 4) Outdoor seating area, plaza, or other interactive landscaped area adjacent to the building that is specifically identified

and/or covered, and approved by the review authority; and/or

- 5) Other similar facade variations approved by the review authority.

Figure 14.____-__: Facade Variation



- b. Base, Middle, and Top of Building.

All non-industrial buildings shall have a clear and distinct base, middle and top to break up vertical mass (Figure 14.____-__). All facades visible from a right-of-way or River Trail shall utilize horizontal bands and/or changes in color, material, form and/or pattern to differentiate the base, middle, and top of the building, subject to the following requirements:

- 1) Horizontal bands or other changes in pattern or material shall be a minimum of 8 inches high (the length of a standard brick) and shall project a minimum of one (1) inch from the building face.
- 2) Changes in building massing and form may also be used to differentiate a building's base, middle, and top. This may include architectural setbacks or projections, measuring a minimum of 3 inches.

Figure 14.____-__: Base, Middle & Top of Building



c. Parking Location.

Parking and vehicle maneuvering areas shall not be located between the front building facade and the front property line, or between a building facade facing the River Trail and the property line adjacent to the River Trail.

Parking shall be permitted between a building and an interior lot line that is not a rear lot line, provided the following standards are met:

- 1) Where surface parking or maneuvering areas are located adjacent to a right-of-way or the River Trail, a minimum 5-foot-wide landscaped strip shall be provided between the parking and maneuvering area and the right-of-way or River Trail. The landscaped strip shall be planted with trees spaced not more than 30 feet on center and with a mix of shrubs and ground cover. Additional standards for landscaping in parking areas are found in Section 3.120, 7.170, and 14.120.B.45.
- 2) Parking and maneuvering areas, including accessways and driveways, must not exceed 40 percent of a lot frontage.

4. Guidelines for Non-Industrial Uses

a. Compatibility with Historic Buildings.

- 1) The massing, scale, and configuration of non-industrial buildings should be similar to historic structures that are visible from the public right-of-way within three blocks of the development site.
- 2) Non-Industrial buildings should be compatible with the vertical proportions of historic facades and the simple vertical massing of historic structures that are visible from the public right-of-way within three blocks of the development site.
- 3) The location, size, and design of windows and doors in non-industrial buildings should be compatible with historic structures visible from the public right-of-way within three blocks of the development site.
- 4) Development should be designed so that structures are not substantially different in character from adjacent buildings, in terms of size, mass, or architectural form.

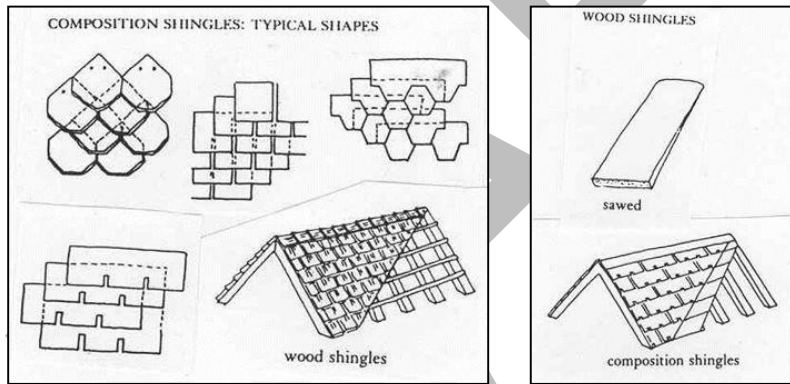
C. Roof Form and Materials.

1. Roof Form Standards for All Uses.

The following roof forms are prohibited:

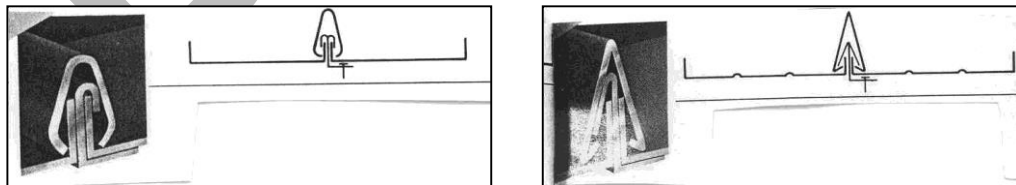
- a. False mansard or other applied forms; and
 - b. Dome skylights.
2. Roof Materials Standards for All Uses.
- a. Buildings shall be constructed or reconstructed with one of the following roofing materials:
 - 1) Cedar shingle (Figure 14.__- __);
 - 2) Composition roofing (Figure 14.__- __); or
 - 3) Materials cited in Section 14.___(C.4) or Section 14.__(C.6).

Figure 14.__- __: Roofing Materials



- b. The following roofing materials are prohibited for all types of buildings:
 - 1) High profile standing seam metal roof (Figure 14.___- __); and
 - 2) Brightly colored roofing material.

Figure 14.__- __: Low (3/8" x 1") and High (1/4" x 1-1/4") Roof Seams



- c. Roofing materials shall be gray, brown, black, deep red, or another subdued color.

3. Roof Form Standards for Non-Industrial Uses

Buildings for non-industrial uses shall include one of the following roof forms:

- a. Single gable with low pitch; or
- b. Repetitive gable with steep pitch; or
- c. Flat or gable roof behind parapet wall (Figure 14.____- _).

Figure 14.____- _: Non-Industrial Building, Flat Roof Behind Parapet Wall



4. Roof Materials Standards for Non-Industrial Uses.

Buildings for non-industrial uses shall be constructed or reconstructed with one of the following roofing materials:

- a. Materials cited in Section 14.____(C.2); or
- b. Built-up roofing materials.

5. Roof Form Standards for Industrial Uses.

Buildings for industrial uses shall include the following roof forms:

- a. Single gable with low pitch; or
- b. Repetitive gable with steep pitch (Figure 14.____- _ and Figure 14.____- _); and
- c. Shallow eaves (Figure 14.____- _).

Figure 14.____- __: Roof Pitches

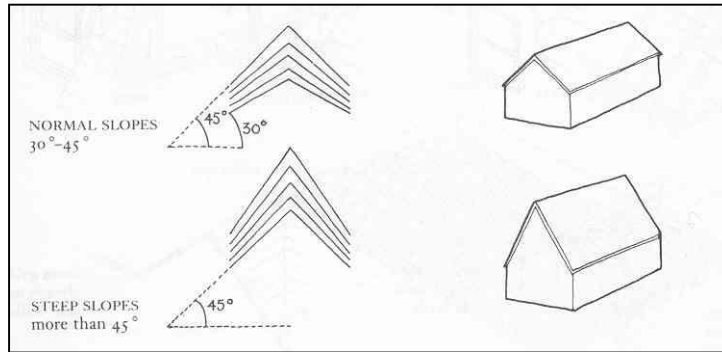
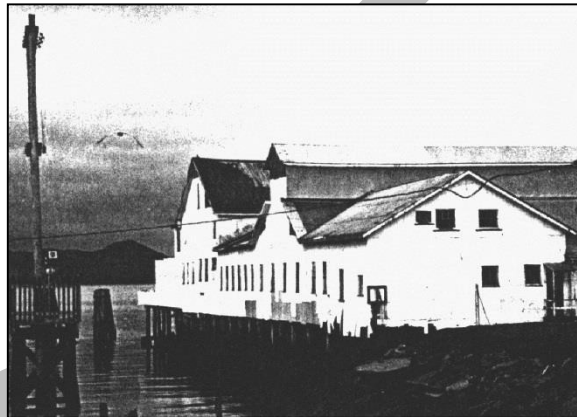


Figure 14.____- __: Industrial Building, Multiple Gables, Monitor Roof, and Shallow Eaves



6. Roof Materials Standards for Industrial Uses.

Buildings shall be constructed or reconstructed with one of the following roofing materials:

- a. Materials cited in Section 14.____(C.2); or
- b. Galvanized corrugated metal; or
- c. Low profile standing seam, metal roof (Figure 14.____- __); or
- d. Roll down.

7. Roof Form Guidelines for Non-Industrial Uses.

Buildings for non-industrial uses may also include the following roof forms or features:

- a. Structural skylights
- b. Shallow eaves behind parapet wall

8. Roof Form Guidelines for Industrial Uses.

Buildings for industrial uses may also include one or more of the following roof forms or features:

- a. Small shed roof dormers
- b. Monitor roof on ridge line (Figure 14.____- _)
- c. Flat panel skylights or roof window

D. Doors.

1. Standards for All Uses.

The following types of doors and door treatments are prohibited:

- a. Automatic sliding doors;
- b. Primary entry doors raised more than three feet above sidewalk level;
- c. Doors flush with building facade;
- d. Clear anodized aluminum frames; and
- e. Reflective, opaque, or tinted glazing.

2. Guideline for All Uses.

Building lighting should emphasize entrances.

3. Standards for Non-Industrial Uses.

- a. Solid metal or wood doors with small or no windows are prohibited.
- b. Doors with a minimum of 50% of the door area that is glass are required.

4. Guidelines for Non-Industrial Uses.

- a. Doors should be recessed (Figures 14.__- _and 14.____- _).
- b. Large cafe or restaurant doors that open the street to the interior by pivoting, sliding, or rolling up overhead are encouraged (Figure 14.____- _).
- c. Well-detailed or ornate door hardware is encouraged (Figure 14.____- _).
- d. Contemporary hardware should be compatible with the design of the door.

- e. Transom, side lites, or other door/window combinations are encouraged (Figure 14.____- _).
- f. Doors combined with special architectural detailing are encouraged.
- g. Double or multiple door entries are encouraged (Figure 14.____- _).

Figure 14.____- _: Roll-Up Doors and Recessed Doors



Figure 14.____- _: Recessed Doors, Contemporary Door Hardware, Single/Double Doors, Side Lites, and Transom Windows



E. Windows.

1. Coverage Standards for All Uses.

- a. All building facades visible from a public right-of-way or the River Trail shall have windows or other openings in the facade, except as noted in subsection (E.1.b) of this section. Blank walls on any facades visible from the right-of-way or River Trail for any type of use are prohibited.

b. Exception for elevator shafts.

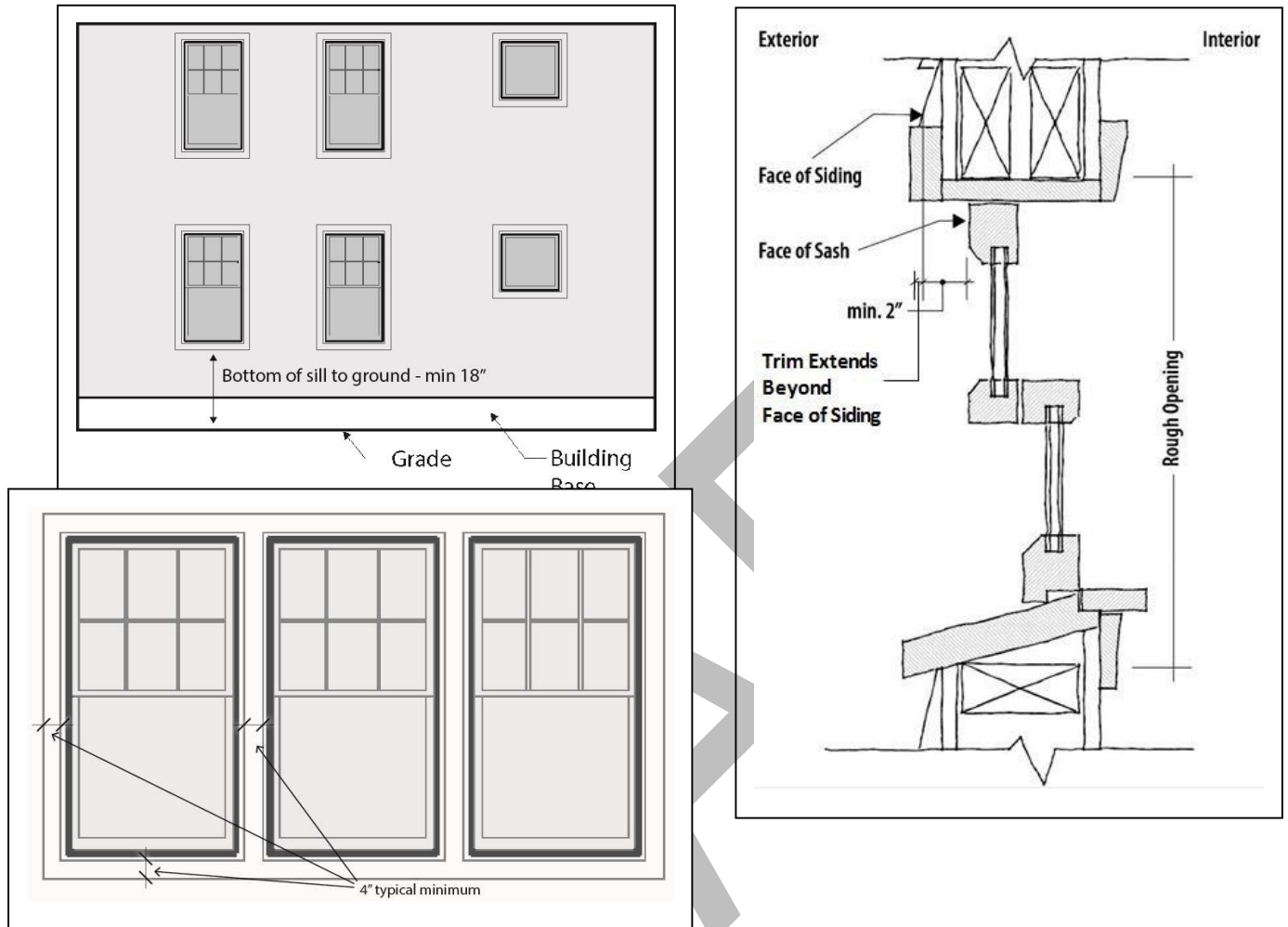
An exception to the window coverage percentage standard may be allowed for the portion of a building facade that includes an elevator shaft with the inclusion of architectural detail / design features in amounts equal to the minimum window coverage requirement. Such architectural details shall include but not be limited to a change in material, horizontal projections, engaged columns or pilasters, belt course, moldings, clock, or other similar features to avoid blank walls.

2. Design Standards for All Uses.

a. Window detailing. Windows shall have casings/trim, sills, and crown moldings. Window detailing shall meet the following requirements.

- 1) Casings/trim shall have minimum dimensions of 5/4 inch x 4 inch and shall extend beyond the facade siding. Exceptions may be granted.
- 2) Windows shall be recessed a minimum distance of two (2) inches from the trim surface to ensure a shadow line/effect.
- 3) The bottom of the sill shall be a minimum of 18 inches above the ground or floor elevation.

Figure 14._____-_: Window Detailing – Trim and casement location and dimensions

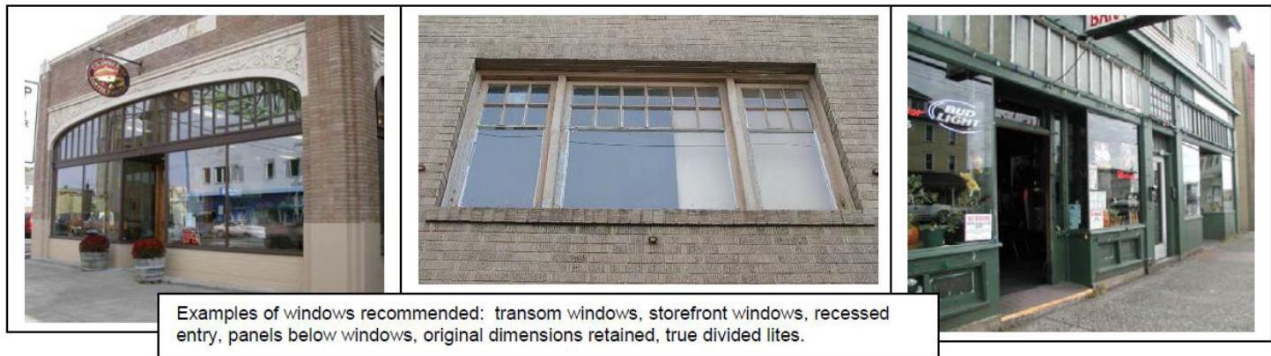


b. The following types of windows or window treatments are prohibited:

- 1) Residential-styled window bays;
- 2) Half-round windows;
- 3) Tinted and/or reflective glass;
- 4) Sliding windows;
- 5) Vinyl windows; and
- 6) Blocked-out windows; and
- 7) Windows that extend beyond the plane of the building facade.

3. Design Guidelines for All Uses.
 - a. Windows, including transoms on existing buildings, should retain their original size and location as part of renovation activities.
 - b. Windows that open by pivoting, casement, single hung, or other shuttering are encouraged.
 - c. Painted wood or stucco panels or tile clad panels below windows are encouraged (Figure 14.____- _).
 - d. Clear glass is encouraged.
 - e. True divided lites are encouraged (Figure 14.____- _). Simulated divided lites shall have exterior muntins to create exterior shadow lines.
 - f. Boldly articulated window and storefront trim are encouraged.

Figure 14.____- _: Transom Windows, Panels Below Windows, and True Divided Lites



4. Coverage Standards for Non-Industrial Uses.
 - a. West Gateway Subarea.

At least 40% of the ground-floor facades of non-industrial uses visible from a right-of-way and/or River Trail shall be covered by windows. At least 30% of the upper-floor facades visible from a right-of-way and/or River Trail shall be covered by windows, except as noted in subsection (c) of this section.
 - b. Core Subarea.

At least 50% of the ground-floor facades of non-industrial uses visible from a right-of-way and/or River Trail shall be covered by windows. At least 30% of the upper-floor facades visible from a right-of-way and/or River Trail shall be covered by windows, except as noted in subsection (c) of this section.
 - c. Exception for elevator shafts.

An exception to the window percentage may be allowed for the portion of a building facade that includes an elevator shaft with the inclusion of architectural detail / design features in amounts equal to the minimum window coverage requirement. Such architectural details shall include but not be limited to change in material, horizontal projections, engaged columns or pilasters, belt course, moldings, clock, or other similar features to avoid blank walls.

5. Coverage Standards for Industrial Uses.

- a. All facades of buildings for industrial uses in the Uniontown Overlay Zone that are visible from a public right-of-way and/or River Trail, and/or the Columbia River shall have windows. However, buildings for industrial uses are not subject to minimum window area requirements.
- b. Buildings for industrial uses are not required to have ground floor windows but shall have, at the least, clerestory or transom windows on the upper story facades or above a height of 14 feet.

F. Siding and Wall Treatment.

1. Standards for All Uses.

The following types of siding and wall materials and treatments are prohibited:

- a. Cladding materials such as corrugated metal panels or spandrel glass;
- b. Panels that are poorly detailed or do not have detailing;
- c. Neon or other fluorescent colors;
- d. Bright or primary wall colors for the entire wall surface;
- e. Flagstone, simulated river rock, or other similar veneer cladding;
- f. Painted brick; and
- g. Non-durable materials such as synthetic stucco or shingles at the ground floor.
- h. Textured fiber cement siding. Smooth fiber cement siding is allowed.

2. Guidelines for All Uses.

- a. Variations in wall cladding materials and patterns consistent with historic patterns are encouraged (Figure 14.____- _).
- b. Natural or subdued building colors are encouraged (Figure 14.____- _).

- c. Bright colors may be used for accent trim, not to exceed 15% of the area of any facade.
- d. Durable materials such as brick, stucco, granite, pre-cast concrete, board and batten, or horizontal wood siding should be used (Figure 14. - _). These materials include galvanized corrugated metal on buildings for industrial uses.
- e. Architectural wall features such as belt courses, pilasters, and medallions are encouraged.

Figure 14.____- _: Siding Variety and Compatible Materials and Colors



G. Awnings.

1. Standards for Types of Awnings and Treatments.

- a. Awnings over building entries shall be a minimum of 5 feet deep. Awnings over windows shall be a minimum of 3 feet deep. The bottom of all awnings shall be 8 to 12 feet above grade.
- b. The following types of awnings and awning treatments are prohibited:
 - 1) Fixed “bubble shaped” awnings (Figure 14.____- _); and
 - 2) Awnings lit internally.

2. Guidelines for Types of Awnings and Treatments.

- a. Vinyl or other non-compatible material awnings are discouraged (Figure 14.____).

Figure 14._____-__: Prohibited and Discouraged Awning Types and Treatments



3. Standards for Awning locations Along River Trail and North/South Rights- of-Way.

Awnings are generally discouraged and shall not project into the setback and/or stepback areas.

H. Lighting.

1. Standards for Lighting Types and Treatments for All Uses.

The following lighting types or treatments are prohibited:

- a. Neon silhouette accent lighting;
- b. Fluorescent tube lighting;
- c. Security spotlight;
- d. Signs lit by lights containing exposed electrical conduit, junction boxes, or other electrical infrastructure; and
- e. Up-lighting that shines into the sky or light that shines into other properties or rights-of-way.

2. Standards Regarding Lighting Glare for All Uses.

All uses shall comply with applicable lighting standards in Section 3.128.

3. Guidelines Regarding Wall-Washing Light.

Wall-washing lighting fixtures should be concealed and integrated into the design of buildings or landscape walls and stairways (Figure 14._____-__). Wall-washing lighting should be designed to minimize light directed upwards into the night sky.

4. Guidelines for Lighting Types and Treatments for Non-Industrial Uses.

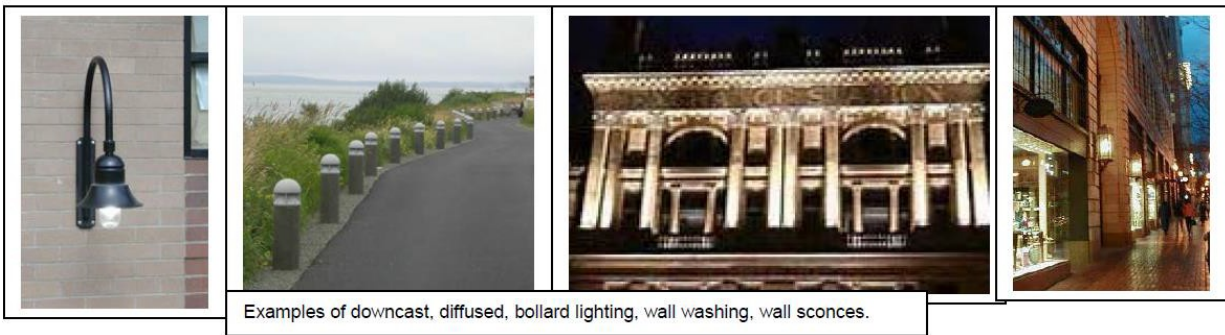
The following lighting types or treatments are encouraged.

- a. Decorative lighting integrated with architecture.
 - b. Historic street lamps along walks and parking lots.
5. Guidelines for Lighting Types and Treatments for Industrial Uses.

The following lighting types or treatments are encouraged.

- a. Industrial pan light with goose neck.
- b. Low bollard lighting.

Figure 14.____- __: Downward and Diffused Lighting, Wall-Washing Lighting



I. Signs.

Signs in the Uniontown Overlay Zone are subject to the requirements in Article 8 (Sign Regulations) of the Astoria Development Code. The following additional standards and guidelines apply to signs in the Uniontown Overlay Zone.

1. Sign Standards for All Uses.

- a. Monument signs (Figure 14.____- __) are allowed up to a maximum of 32 square feet.
- b. Monument signs shall be a maximum of five (5) feet tall.
- c. Monument signs shall be constructed from materials that are consistent with the historic character of the area, including wood, brick, stone, and metal.
- d. Freestanding pole-mounted signs are prohibited (Figure 14.____- __).

2. Sign Guidelines for All Uses.

The following sign types are encouraged.

- a. Hanging blade signs.
- b. Signs painted on building facade.

- c. Signs applied to building facade.
- d. Front lit.
- e. Graphics historic in character.

Figure 14.____: Monument Signs and Freestanding Pole-Mounted Signs



14.____. LANDSCAPING.

Landscaping is required in the Uniontown Overlay Zone in accordance with the provisions in this Section and those in Sections 3.120 to 3.125, and 7.170. The provisions in this Section apply to new construction or exterior renovations with a value of at least 20% of the assessed value of the structure, or in the event of installation of new parking areas.

A. Minimum Landscaped Area.

1. West Gateway Subarea.

- a. A minimum landscaped area of 15 percent of the total lot area shall be provided in the West Gateway Subarea in accordance with the standards of Section 14.____ [landscaping standards section];
- b. Landscape areas must be visible from the public right-of-way and/or River Trail to count toward the minimum landscape area requirement.

2. Core Subarea.

No minimum landscaped area shall be required in the Core Subarea. Parking lots shall be landscaped in accordance with Section 7.170 and Section 3.105 through 3.120.

B. Landscape Standards.

Where landscaping is provided, the following minimum planting and coverage standards shall apply. These standards apply in addition to the landscaping standards of Section 3.105 and Section 3.125.

1. One (1) tree shall be provided for every 600 square feet of required landscaped area.
2. One (1) evergreen shrub having a minimum mature height of 48 inches shall be provided for every 400 square feet of required landscaped area.
3. All landscape areas, whether required or not, that are not planted with trees and shrubs or covered with allowable non-plant material, shall have ground cover plants that are sized and spaced to achieve plant coverage of not less than 75 percent at maturity.
4. Bark dust, chips, aggregate, or other non-plant ground covers may be used, but shall cover not more than 25 percent of any landscape area. Non-plant ground covers cannot be a substitute for required ground cover plants.
5. Adjacent to the River Trail - Land Side or Upland Standards

The following standards apply to landscaping along the frontage of parcels abutting the River Trail to the south.

- a. Maximum spacing of trees.
 - 1) 20 feet on center for non-industrial uses
 - 2) 15 feet on center for industrial uses
 - b. Maximum spacing of shrubs
 - 1) Five (5) feet on center for non-industrial uses
 - 2) Three (3) feet on center for industrial uses
 - c. Ground cover landscaping is required in between shrubs and trees.
 - d. Trees shall not exceed 35 feet in height at maturity
6. Landscaping Credits for Non-Vegetation Features.
 - a. The Community Development Director may approve non-vegetative features to account for up to 25% of required landscaping when the features consist of the following:
 - 1) Hardscaped pedestrian-oriented areas (e.g., courtyards, plazas); and/or
 - 2) At least one of the following amenities meeting the City approved design within the public right-of-way:
 - (a) bike rack

- (b) bench
- (c) table
- (d) drinking fountain
- (e) directional or interpretive/information signage
- (f) trash or recycling container
- (g) lighting
- (h) restroom

Permeable paving and other stormwater management techniques are encouraged in the design of these areas.

- b. An application proposing more than 25% of required landscaping be credited by non-vegetative features is subject to approval in accordance with procedures in Article 9 and Article 12.
- c. Non-vegetative features allowed in the public right-of-way in lieu of required landscaping shall be maintained by the applicant. There shall be a maintenance agreement or other City approved agreement. Failure to maintain or loss of the non-vegetative feature will result in the requirement for installation of the landscaping in accordance with the Code at the time of the loss.

C. Street Trees.

Street trees shall be planted within the right-of-way along both sides of the street on all streets in the Uniontown Overlay Zone in accordance with the provisions in this Section ____.

- 1. Spacing should be 30 feet on center, depending on species and branching habit.
- 2. Minimum size of deciduous trees should be 2" caliper, with an upright form.
- 3. Mature branching height should be a minimum of 15 feet.
- 4. Required street trees shall be maintained by the adjacent property owner and/or other identified entity. There shall be a maintenance agreement or other City approved agreement.

14.____. OFF-STREET PARKING.

In the Uniontown Overlay Zone, the following provisions apply to parking requirements established in Article 7 of this Code.

A. Reductions.

Minimum number of parking spaces required in Section 7.100 may be reduced by 50% for uses with less than 5,000 square feet of gross floor area. Reductions meeting these requirements shall be processed as a Type I Administrative Permit.

B. Exceptions.

Exemptions from minimum number of parking spaces required in Section 7.100 are permitted under the following conditions:

1. Existing buildings that cover the majority area of the site with insufficient open area for off-street parking spaces; and/or
2. Building expansions of 10% or less which do not decrease available off-street parking spaces on the site; and
3. Exemptions shall be processed as a Type I Administrative Permit. Exceptions from off-street parking that do not meet the above criteria shall be processed as a Variance in accordance with Article 12.

Section 2. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ADOPTED BY THE COMMON COUNCIL THIS _____ DAY OF _____, 2019.

APPROVED BY THE MAYOR THIS _____ DAY OF _____, 2019.

ATTEST:

Mayor

Brett Estes, City Manager

ROLL CALL ON ADOPTION: YEA NAY ABSENT

Commissioner	Rocka Brownson Herman West
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Mayor Jones

ORDINANCE NO. __-

AN ORDINANCE AMENDING THE ASTORIA COMPREHENSIVE PLAN PERTAINING TO ADOPTION OF THE ASTORIA UNIONTOWN REBORN MASTER PLAN AS A BACKGROUND PLAN AND STUDY AND IMPLEMENTATION POLICIES

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Astoria Comprehensive Plan Section CP.028.K pertaining to Uniontown Reborn Master Plan is hereby added to read as follows:

“K. Astoria Uniontown Reborn Master Plan, adopted by the City Council on **** by Ordinance No. **.

Section 2. Astoria Comprehensive Plan Section CP.028.L pertaining to Uniontown Reborn Master Plan addendum to the Astoria Transportation System Plan, adopted by the City Council on April 21, 2014 by Ordinance 14-02, is hereby added to read as follows:

“L. Uniontown Reborn Master Plan Addendum to the 2014 Astoria Transportation System Plan, adopted by the City Council on **** by Ordinance No. **.”

Addendum document attached and incorporated as part of this Ordinance.

Section 3. Astoria Comprehensive Plan Section CP.037 pertaining to Port-Uniontown Overlay Area is hereby amended to read as follows.

CP.037. ~~Port-Uniontown Overlay Area~~ Uniontown Area and Overlay.

~~The Port-Uniontown Overlay Area is generally located along the Astoria Waterfront. The District boundaries extend from the Smith Point Roundabout to the Columbia/Bond intersection, from properties fronting on the south side of West Marine Drive (US 101/US 30) to and including the Columbia River. The exact area is shown in Figure 1.2, and was originally created to coincide with the boundaries of the Astor West Urban Renewal Area, created in late 2002.~~

~~The Port-Uniontown Overlay Area is defined by the Columbia River waterfront and West Marine Drive. Existing uses associated with the waterfront include Port of Astoria operations and offices, other marine industrial sites, a marina, a hotel, and the River Trail shared-use path. Existing uses associated with West Marine Drive feature a mix of single- and multi-family residences, commercial services (including gas stations, bars and restaurants, hotels, and a market), and institutional uses such as a fire station and an ODOT facility.~~

~~Between the years of 2001 and 2006, areas of the Port-Uniontown Overlay Area were the subject of a series of planning efforts by the Port of Astoria. These earlier plans divided the waterfront into two districts: the western industrial-oriented Marine Service Center District and the eastern visitor and recreation-oriented Marina District. They envisioned development of a conference center in conjunction with the existing motel site (400 Industry), which, in part, spurred the formation of the Astor West Urban Renewal Area. The Port/Uniontown Transportation Refinement Plan was adopted by Ordinance 07-01 on February 20, 2007.~~

~~The Astor West Urban Renewal Plan, adopted in December 2002, was created to support redevelopment of former industrial sites within Uniontown, development of a conference center, and transportation and recreation improvements including extending the River Trail, reconstructing trolley tracks, building streets for more connectivity, and enhancing streetscapes with lighting, seating, and landscaping. The Port Uniontown City of Astoria Comprehensive Plan GP.038 Area Descriptions and Policies – 3 Transportation Refinement Plan, adopted in February 2007, developed transportation, access, and circulation improvements for roads and paths in the Overlay Area, with particular focus on West Marine Drive. The land use vision that evolved from the Refinement Plan process is the basis for the Port Uniontown Overlay Area.~~

~~The Port Uniontown Overlay Area is comprised of eight subdistricts with distinct character largely reflected in their names. The first two subdistricts are waterfront subdistricts identified in earlier plans, and the other six subdistricts focused around West Marine Drive were products of the visioning process conducted for the Port/Uniontown Transportation Refinement Plan. The eight subdistricts include:~~

- ~~1. Marine Service Center District~~
- ~~2. Marina District~~
- ~~3. Tourist/Visitor Oriented District~~
- ~~4. Neighborhood/Visitor Services District~~
- ~~5. Marine Services/Industrial District~~
- ~~6. Neighborhood Corridor District~~
- ~~7. Gateway/Open Space District~~
- ~~8. Highway Corridor District~~

Located along the Columbia River, in the northwest corner of the City of Astoria, the Uniontown Neighborhood is both a gateway into the City and an important industrial and commercial activity center. The City's iconic 4.1-mile-long Astoria-Megler Bridge is located in Uniontown, which brings people across the Columbia River from Washington. People from the Oregon coast access Uniontown by crossing the New Young's Bay Bridge from the west. The historic past of a thriving cannery and seafood port is still apparent today in Uniontown with the location of the Port of Astoria along the waterfront.

The Uniontown Area ~~It~~ slightly overlaps with the West End General Land Use Area, an established residential neighborhood addressed in Sections CP.030 through CP.035. There is also overlap with the Uniontown-Alameda National Register Historic District, placed on the National Register of Historic Places in 1988, which extends roughly from West Marine Drive south to West Exchange Street and between Hull Avenue on the west and Hume Avenue on the east. The Area also overlaps with the Astoria Riverfront Vision Plan "Bridge Vista" area which extends along the Riverfront from Pier 1 to approximately 2nd Street. However, the Uniontown Overlay zone does not overlap with the Bridge Vista Overlay zone.

The Uniontown Reborn Master Plan focuses on the portion of West Marine Drive from Smith Point to Columbia Avenue in the City of Astoria. The area includes land adjacent to West Marine Drive as well as land to the north and in the Port of Astoria that is designated for commercial, industrial, and mixed-use development. The existing conditions in the area are summarized below:

- **Land Use Conditions:** The Uniontown area includes a diverse range of land uses. The

existing land uses can be broadly categorized as industrial, commercial, and residential. The area includes a range of types of uses within these three categories, particularly commercial and industrial uses.

- **Economic Conditions:** Uniontown's economic conditions are based on both industrial employment and tourism-related and retail businesses. Housing affordability is a challenge for Uniontown and preserving the historic character of the neighborhood is a top priority among the community.
- **Transportation Conditions:** West Marine Drive is a major, auto-oriented commercial corridor in Astoria, that runs right through Uniontown. High traffic volumes provide Uniontown with lots of visitors and people passing through daily, however. Sidewalks and bicycle facilities exist, but in spots they are narrow or uncomfortable to use. Transit service also exists along this corridor. As West Marine Drive moves east, closer to downtown Astoria, the transportation environment transforms into a more pedestrian friendly street.

Uniontown's historic character and central location are key attributes of the neighborhood, but due in part to a lack of a unifying vision and a coherent set of plans to guide public investments and support redevelopment activity, investment has not made its way into Uniontown like it has for other historic areas of Astoria.

The purpose of the Uniontown Reborn Master Plan is to better integrate transportation and land use planning and develop new ways to support economic development along with safety and access enhancements to improve conditions for pedestrians, bicyclists, transit users, and motorists. The project will lay the groundwork for design and construction of streetscape and lane reconfigurations improvements on West Marine Drive/U.S. 101, along with potential land use and development code refinements to foster community-supported future development.

The Uniontown Reborn Master Plan was developed through a process of identifying and considering multiple alternatives for land uses, transportation, and public improvements. The community and a Stakeholder and Technical Advisory Committee (STAC) provided input and weighed the alternatives against a set of evaluation criteria. The preferred alternative for the plan is summarized below.

- **Land Use Preferred Alternative.** The preferred alternative for land uses in the area focuses change along the West Marine Drive corridor. The alternatives vary across two subareas. The West Gateway Subarea extends from Smith Point to Portway Street. The Plan envisions that this subarea will incrementally transition from an auto-oriented environment into a more pedestrian-oriented and walkable form. The Core Subarea extends from Portway Street to Columbia Avenue, on the south side of West Marine Drive. The Plan envisions that the traditional urban pattern of this area will be preserved and strengthened as properties are improved and new buildings are added in the area. The land use alternative is implemented through the Uniontown Overlay zone, which modifies development code provisions related to use regulations, setbacks, landscaping, building height and massing, and design standards and guidelines.
- **Transportation Preferred Alternative.** The preferred alternative for transportation envisions that West Marine Drive would be reconfigured to create an environment that is more pedestrian- and bicycle friendly and a safer street with fewer crashes, while

continuing to meet ODOT mobility targets. The alternative includes improvements to pedestrian and bicycle facilities, such as widening sidewalks and adding or upgrading bike lanes. The preferred transportation alternative does not designate specific transit enhancements but includes recommendations for providing safe and comfortable access to and from current and future transit stations.

- **Public Improvements Preferred Alternative.** Seven public improvements have been identified by the public to help achieve the vision for Uniontown. These additional improvements support the land use and transportation alternatives of the Plan and are important to creating a safer and more inviting neighborhood for both residents and businesses. The improvements include enhanced pedestrian crossings, lighting improvements, pedestrian and bicycle connections, wayfinding, transit stop improvements, potential off-street parking locations, and gateway opportunities.”

Section 4. Astoria Comprehensive Plan Section CP.038 pertaining to Port-Uniontown Overlay Area Policies is hereby amended to read as follows.

“CP.038. Port-Uniontown Overlay Area Uniontown Area and Overlay Policies.

~~e. The City will use the vision established in the Port/Uniontown Transportation Refinement Plan (2007) to direct future development in the Port- Uniontown Overlay Area. The overall Comprehensive Plan Policies are to:-~~

- ~~e. Promote development that complements the surrounding areas of Downtown and the West End.~~
 - ~~b. Enhance existing primary uses, such as Port of Astoria facilities, the marina, visitor services, open space, trails, and small businesses and neighborhoods.~~
 - ~~c. Support redevelopment of former industrial sites and vacant and underutilized lots~~
 - ~~d. Stimulate development interest by establishing complementary surrounding land-uses and quality development and design, and by improving transportation-conditions through road construction and connections, circulation plans, and access management plans.~~
 - ~~e. Establish visual and physical linkages within and around the Port-Uniontown-Overlay Area, with emphasis on the Columbia River waterfront.~~
 - ~~f. Create a pedestrian-friendly environment through the District by increasing-connectivity throughout the Port-Uniontown Overlay Area, orienting buildings-toward adjacent streets and pathways, extending the River Trail, adding and-improving sidewalks, and enhancing the streetscape with landscaping, human-scale lighting, seating, and other amenities.~~
- ~~2. The City will implement the Port-Uniontown Overlay Area element of the Comprehensive-Plan through its Design Review process and amendments to the Development Code that provide design and development standards.~~

- ~~3. The City, through the Development Code, will develop a set of design standards for the Port Uniontown Overlay Area that address building massing and orientation, architecture, access and parking, streetscape, landscaping and other elements. These standards will apply to development projects in the District as defined in the Development Code.~~
- ~~4. To the extent possible, the design and development standards are intended to be clear and objective so that most proposed development can be evaluated administratively. The Design Review Committee, created and enabled by the Development Code, will review appeals of administrative decisions and proposals that vary from the standards and yet may still embody the spirit of the Port Uniontown Overlay Area.~~
- ~~5. The City encourages public and private owners in the Port Uniontown Overlay Area, especially large landowners such as the Port of Astoria, to continue to participate and collaborate with the City in implementing the objectives and visions established in the Port/Uniontown Transportation Refinement Plan.~~
1. The City will implement the land use vision and goals of the Uniontown Reborn Master Plan by directing future development to:
 - a. Create an attractive western gateway into the City through high-quality site and building design.
 - b. Develop a pedestrian-friendly commercial district by orienting buildings to the street and creating interesting and comfortable street frontages.
 - c. Expand the tree canopy and provide attractive and environmentally friendly site landscaping.
 - d. Design new or rehabilitated buildings to respect the historic patterns and character of the City and the Uniontown-Alameda National Register Historic District.
 - e. Provide a mix of land uses that support a vibrant commercial corridor, new investment, and a range of employment opportunities.
2. The City will implement the transportation vision and goals of the Uniontown Reborn Master Plan to:
 - a. Reconfigure the cross-section of West Marine Drive to create a more pedestrian and bicycle-friendly street while maintaining mobility and reliability for drivers.
 - b. Upgrade pedestrian and bicycle facilities throughout the Plan area to provide a more comfortable and safer environment for all users.
 - c. Support improvements that provide safer and more comfortable access to and from current transit stops in the area.
3. The City will implement public improvements in the Uniontown area to:
 - a. Enhance the safety of pedestrian crossings and the connectivity of pedestrian and

bicycle routes throughout the plan area.

- b. Improve street lighting to increase visibility while preserving the historic patterns and character of the area.
- c. Create a more inviting commercial district and neighborhood by installing a wayfinding system that guide people to points of interest and important destinations.
- d. Actively seek out opportunities to develop public, off-street parking facilities in the district in order to reduce reliance on private off-street parking lots and on-street parking.
- e. Design and install gateway elements to welcome visitors to the City of Astoria and contribute to Uniontown's "working waterfront" history and identity."

Section 5. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ADOPTED BY THE COMMON COUNCIL THIS _____ DAY OF _____, 2019.

APPROVED BY THE MAYOR THIS _____ DAY OF _____, 2019.

ATTEST:

Mayor

Brett Estes, City Manager

ROLL CALL ON ADOPTION:

YEA

NAY

ABSENT

Commissioner

Rocka
Brownson
Herman
West

Mayor Jones

ORDINANCE NO. __-

AN ORDINANCE AMENDING THE ASTORIA LAND USE AND ZONING MAP PERTAINING TO DESIGNATION OF THE ASTORIA UNIONTOWN OVERLAY ZONE

THE CITY OF ASTORIA DOES ORDAIN AS FOLLOWS:

Section 1. Astoria Land Use and Zoning Map is hereby amended by the addition of the Uniontown Overlay Zone as follows:

Location:

Map T8N R10W Section 13, Tax Lots 200, 400, 402, 1400, 1401

Map T8N R9W Section 7CA, Tax Lots 3000, 3200, 3300, 3400, 3500, 3700, 3800, 3900, 4000, 4100, 4200, 4201

Map T8N R9W Section 7CC, Tax Lots 2600, 2800, 2900, 3100, 3400, 3500, 6300, 6500, 6700, 6800, 6900, 7000, 7100, 7300, 7400, 7600, 7700, 7800, 7900, 8000, 8100, 8200, 8201, 8300, 8500, 8900, 9100, 9200, 9300, 9400, 9500

Unplatted lots fronting Block 8, Taylor

Lots 19 to 36, Block B, Taylor

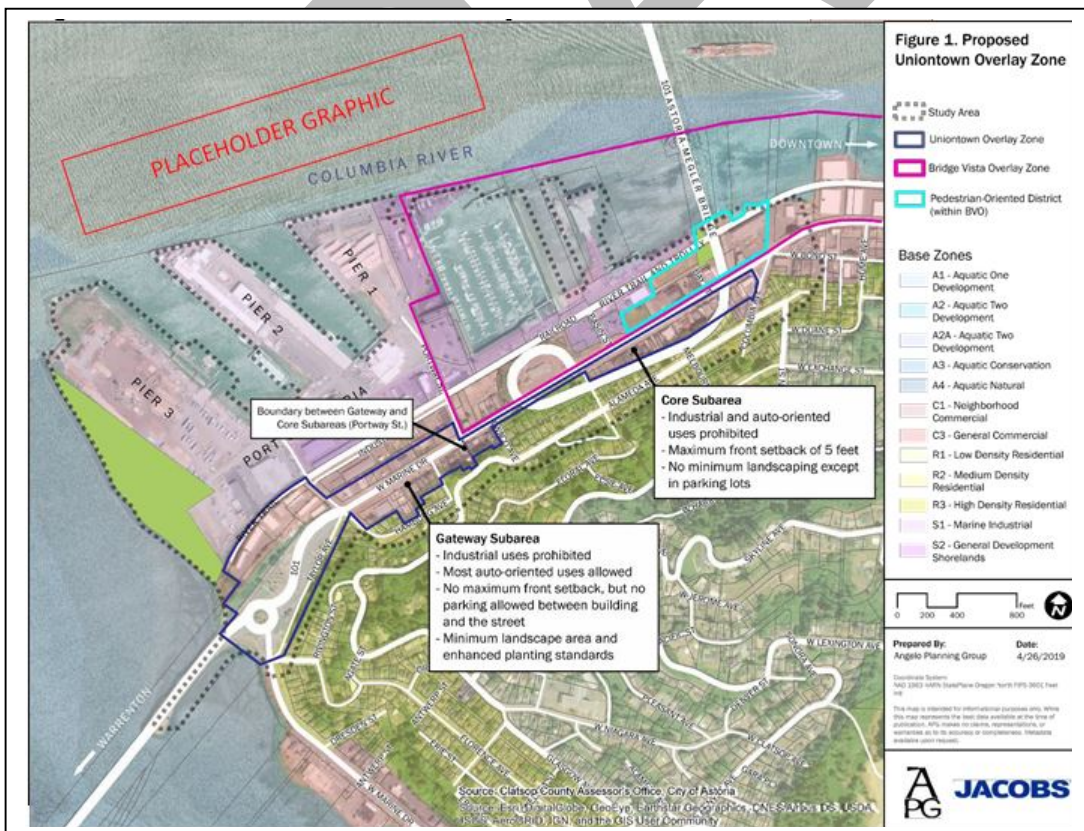
Lots 1 to 22, 27 to 34, north 50' Lots 35 to 36, Block 6 Taylor

North portion Lots 1 to 8, Block 5, Taylor

Lots 1 to 24, Block 3, Taylor

Lots 1 to 22, Block 2, Taylor

Rights-of-way and vacated rights-of-way within the Overlay Zone boundary



Section 2. Effective Date. This ordinance and its amendment will be effective 30 days following its adoption and enactment by the City Council.

ADOPTED BY THE COMMON COUNCIL THIS ____ DAY OF _____, 2019.

APPROVED BY THE MAYOR THIS ____ DAY OF _____, 2019.

ATTEST:

Brett Estes, City Manager

Mayor

ROLL CALL ON ADOPTION:

YEA

NAY

ABSENT

Commissioner

Rocka
Brownson
Herman
West

Mayor Jones

DRAFT